

Ward: Radcliffe - West

Item 03

Applicant: Hive Homes (Greater Manchester) LLP

Location: Site of the former Riverside High School, School Street, Radcliffe, M26 3BW

Proposal: Residential development to erect 90 dwellings with associated parking and landscaping

Application Ref: 69233/Full

Target Date: 19/04/2023

Recommendation: Minded to Approve

It is recommended that this application is Minded to Approve subject to the signing and completion of a Section 106 agreement for recreation provision, affordable housing and off-site biodiversity mitigation, thereto in accordance with policies H2/1, RT2/2 of the adopted Unitary Development Plan, SPD1 and SPD5 and Section 15 of the National Planning Policy Framework. If the agreement is not signed within a reasonable timeframe, then delegated authority is sought by the Development Manager to determine the application.

The Development manager requests a site visit.

Description

The application site relates to 2.3ha of land which formally comprised Radcliffe High School site and its grounds. The main school building has long since been demolished and the northern part of the site redeveloped for a school for children with special educational needs, known as Millwood School, which was granted planning permission in 2010. As part of that application, permission was also granted in outline to develop the southern part of the site for a residential development. This permission was not implemented.

The site is bounded by a builder's merchant to the north, the Millwood School to the west, traditional terraced housing to the east, single storey sheltered housing and more contemporary suburban housing opposing the site to the south. The site falls within the Radcliffe Strategic Regeneration Framework and as such is a priority housing site.

The proposal is for full planning permission for a residential development comprising 90 no. two storey dwellings with associated parking and landscaping. There would be two vehicular accesses into the site, one from School Street on the southern boundary and another from Abden Street at the eastern boundary and two additional dedicated pedestrian from School Street and Ulundi Street. The dwellings would be made up of:

- 30 no. 3 bed mews;
- 6 no. 4 bed mews;
- 38 no. 3 bed semi-detached;
- 10 no. 3 bed detached;
- 6 no. 4 bed detached.

There would be 7no. house types (13 variations in total), of which all would be two storey, although the Newbold mews property would have accommodation in the roof space, served with a small dormer at the front (with a higher ridge).

The finishing materials are indicated as red brick, with blue brick detailing and grey roof tiles with grey uPVC window frames, doors and rainwater goods, but the application states that precise details would be agreed by condition.

Existing boundary iron railings to School Street/Ulundi Street would be retained where possible. Rear gardens to plots would be enclosed by 1.8m high timber fencing and 1.8m brick wall and piers with timber panel infills. An acoustic fence would be erected along the northern boundary to the neighbouring builder's merchant. The existing western boundary fence to the school would be retained.

The proposal has been amended to facilitate the retention of two stands of mature trees arranged in rows northwards from the boundary with School Street. This resulted in the realignment of the access road into the site from School Street westwards. The arrangement of dwellings fronting School Street has also been amended in addition to minor alterations in response to feedback from the Highway Officer.

Relevant Planning History

66307: Demolition and Site Enabling Works. Approved 22/02/2021

53879: Non Material Amendment following the grant of permission 52038. Approved 1/7/2011

52038: Full planning for construction of a new primary school for children with special educational needs together with the construction of associated parking and external play areas; outline proposals to develop the remaining high school site for housing. Approved 16/02/2010

45673: Outline application - residential development including associated infrastructure & open space. Approve with Conditions 14/09/2007

Publicity

Direct neighbour notification undertaken 19/01/2023

Press Notice undertaken 26/01/2023

Site Notice 23/01/2023

5 representations were received raising matters that can be summarised as:

Access/Highways/Parking

- The proposal would add more traffic onto the local road network. School Street is a narrow and very busy road, serving Millwood School with its associated heavy traffic at drop-off and collection times and vehicles only have one way in and out. Additionally, there are horse riders, cyclists and pedestrians accessing the canal and rural paths.
- The junction at Hutchinson Way and Sion Street is already challenging and can be blocked.
- A full traffic study of the area should be undertaken
- Exit via Abden Street is hampered by the traffic lights at Blackburn Street and access via Lord Street would cause congestion given that this isn't light controlled.
- Site traffic would need to be accommodated on site. Traffic associated with the previous demolition blocked pavements and parked poorly.
- Traffic accessing the site from School Street has previously caused damage to pavements and come close to damaging private property as the turning circle is not big enough for larger construction vehicles (some vehicles have had to unload on the street). Such impacts would need to be mitigated.
- The provision of 192 parking spaces should mitigate against overflow parking and having two access points, plus additional pedestrian accesses is good news, as it would split the pedestrian and vehicular traffic, avoiding it all being routed along School Street. Construction activity would need to mitigate against such impacts.
- The parking area to the neighbouring care home should not be blocked by construction/loading/deliveries.
- The documents provided contain a number of inaccuracies - we are assured this will be

understood by both highways & planning in the assessment. The highways officer noted that School Street is the most direct route from the Key Route Network, albeit there are a number of accesses on the opposite side of the street to consider.

- It is stated that the plan therefore shows the adjacent access points, including School Court which is situated approx 25m to the east, low-rise sheltered housing (this is not School Court).

Amenity

- Construction activity should be time limited to mitigate against noise and disturbance.
- The site should be made secure (fencing) to prevent anti-social behaviour.
- The main site entrance should not be from School Street as previously it has caused massive disruption in relation to noise and privacy .
- The neighbouring Millwood School is a Special School to children with complex medical and/or behavioural needs. Construction activities can be very distressing and disruptive to the children (as was found with the remediation of the land).
- Noise and construction activities would need to be managed appropriately to ensure that impacts on the neighbouring care home is kept to a minimum

Ecology/landscaping

- All existing trees would be removed and these provide homes for wildlife and are attractive. The replacement planting must be required, although it would be many years before they provide the same benefits.

Drainage

- The drainage on School Street is poor. There has been sewage floods at neighbouring properties and United Utilities have said that the drainage system is not in good condition. The drainage system would need to be upgraded.

Other matters

- The location of the proposed development indicates that the rear gardens of some houses directly border the neighbouring school site and therefore directly overlook children's playgrounds and the school car park. This could present a safeguarding issue for the school. What measures are in place to provide a barrier against this intrusion?
- The development has the potential to significantly improve the quality of life and recreational leisure facilities for the local community, residents and pupils. Section 106 monies from the developer should be directed to improving the facilities along and access to the towpath of the canal.

Support points

- The development looks well laid out and the properties seem to fit in well with the existing character. The landscaped area fronting School Street would provide a good division.
- New housing and therefore families in the neighbourhood, will create opportunities for the care home to receive new service users in future. Staff are mostly local too, and so this may aid recruitment.

Due to amendments to the proposal, further direct neighbour notification was undertaken on 21/07/2023

2 further representations were received raising the following matters:

- The developer appears to have not given due consideration to the existing properties in the vicinity, overstating the appropriateness of the proposal based on inaccurate information.
- The proposed layout & transport plan neglects to identify the neighbouring properties and two further access points.
- The revised access location places the primary access to the site directly opposite existing property. This will have a huge negative impact on the enjoyment of opposing property and it's resale attractiveness/value.

- Vehicles exiting the estate will cause light nuisance to the main living and sleeping areas at opposing property. If this was triggered by an existing domestic or business property, we would have just cause to claim this as a statutory light pollution nuisance - I feel that developer proposals should be held accountable in the same regard.
- The placement of the junction means, according to the highway code, occupants or visitors to properties opposing are unable to park opposite this junction - it is an offence if hindering emergency services.
- This currently occurs on a daily basis by visitors, deliveries and 'home to school transport', which includes disabled visitors.
- It is suggested that a primary access point opposite Pendlebury Street, potentially with a mini-roundabout, would be far more suitable to the traffic flow and cause significantly less hazard in the area.

Statutory/Non-Statutory Consultations

Traffic Section: No objection, subject to conditions relating to the formation of accesses (including relocation of speed cushions), footpaths, street lighting, review of waiting restrictions, street lighting, adherence to adoption standards (if necessary), agreement of a Construction Traffic Management Plan (CTMP), provision of vehicular turning facilities, driveways and surfacing and waste and recycling provision.

Environmental Health - Contaminated Land: No objections, subject to the inclusion of conditions relating to contaminated land.

Environmental Health - Pollution Control: No objection, subject to condition securing the mitigation measures set out in the Ambient Noise Assessment

Environment Agency: No comments received.

The Coal Authority: No objection.

Waste Management: No objection.

United Utilities (Water and waste): No objection, subject to a condition relating to foul and surface water drainage.

Greater Manchester Police - designforsecurity: No objection. The proposal, where feasible, should be designed and constructed in accordance with the recommendations and specification set out in the supporting Crime Impact Statement.

Transport for Greater Manchester: No objection.

TfGM would recommend that the development funds improved pedestrian facilities at the junction of Pilkington Way / Sion Street / Dale Street and that this is secured by way of planning condition subject to agreement with the Local Highway Authority (LHA).

The site would be served by a new priority junction on School Street. The access should be designed to ensure the provision of tactile paving and dropped kerbs. Any redundant access points which served the former site should be reinstated as continuous footway to adoptable standards. A secondary access is also proposed connecting to Abden Street. This connection should be designed in accordance with adopted standards in liaison with the Local Highway Authority.

TfGM would suggest that it may be beneficial for a review to be undertaken of the Traffic Regulation Orders in the vicinity of the development, with a view to introducing additional parking restrictions as appropriate, as well as ensuring adequate parking restrictions remain in place, and are refreshed accordingly

Nearest bus stops to the site, which provide a range of services at regular intervals, are

around a 4 minute walk from the site and Radcliffe Bus Station around 6 minutes away. Radcliffe Metrolink stop is located around an 11 minute walk away.

TfGM would refer to the Local Authority to determine whether the quantum of development triggers the requirement for a Residential Travel Plan. If not, then TfGM would recommend that a Travel Pack is produced for residents. It is therefore considered that there are genuine public transport alternatives available for future residents of the site.

Planning & Building Regs consultation Fire Protection Dept Bury Fire Station (Part B): No objection.

The Fire Service requires vehicular access for a fire appliance to within 45m of all points within the dwellings and the access road should be a minimum of width of 4.5m and capable of carrying 12.5 tonnes. There should be a suitable fire hydrant with 165m of the furthest dwelling.

The Fire Service strongly supports the installation of domestic sprinkler systems as a positive measure to protect persons. At a small cost occupants' can be given the reassurance of a high level of protection.

Borough Engineer - Drainage Section: No comments received.

Greater Manchester Ecology Unit: No objections, subject to the inclusion of conditions relating to reasonable avoidance measures relating to bats. A contribution should be secured for the off-site mitigation for the loss of 2.32 biodiversity units or alternatively, more of the existing trees could be retained, with more ambitious targets for habitat creation on site.

Minerals and Waste Planning Unit (Urban Vision Partnership Ltd): No comments provided.

Pre-start Conditions - Agent has agreed with pre-start conditions

Unitary Development Plan and Policies

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H4/1	Affordable Housing
EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN1/5	Crime Prevention
EN5/1	New Development and Flood Risk
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
EN7	Pollution Control
EN7/1	Atmospheric Pollution
EN7/2	Noise Pollution
EN7/5	Waste Water Management
EN8/2	Woodland and Tree Planting
RT2/2	Recreation Provision in New Housing Development
HT2/4	Car Parking and New Development
HT4	New Development
HT5/1	Access For Those with Special Needs
HT6/2	Pedestrian/Vehicular Conflict
CF1/1	Location of New Community Facilities
SPD1	Open Space, Sport and Recreation Provision
SPD5	DC Policy Guidance Note 5: Affordable Housing

SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
H5	Housing Improvement
H5/1	Area Improvement
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

ASSESSMENT

Principle - Residential

Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

The National Planning Policy Framework (NPPF) should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. The Framework maintains the emphasis on identifying a rolling five year supply of deliverable housing land.

Bury's Strategic Housing Land Availability Assessment sets out the latest housing supply position, which is made up of sites that have an extant planning permission and sites that have potential to obtain planning permission in the future. This shows that there are a number of sites within the Borough with the potential to deliver a significant amount of housing. However, not all of these sites will contribute to the five year supply calculations as many sites will take longer than five years to come forward and be fully developed (e.g. some large sites could take up to ten years to be completed). As such, latest monitoring indicates that the Council is unable to demonstrate a five year supply of deliverable housing land and this needs to be treated as a material factor when determining applications for residential developments.

The National Planning Policy Framework also sets out the Housing Delivery Test, which is an assessment of net additional dwellings provided over the previous three years against the homes required. Where the test indicates that the delivery of housing was substantially below (less than 75%) of the housing requirement over the previous years, this needs to be taken into account in the decision-taking process. The latest results published by the Government show that Bury has a housing delivery test result of less than 75%, and therefore, this needs to be treated as a material factor when determining applications for residential development.

Therefore, in relation to the proposed dwelling, paragraph 11d) of the National Planning Policy Framework states that where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date, planning permission should be granted unless:

- i. The application of policies in the Framework that protect areas, or assets of particular importance, provides a clear reason for refusing the development proposed; or
- ii. Any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework, taken as a whole.

In this case the 'titled balance' applies and planning permission should be granted unless

the above points Para 11(d) i or ii apply.

Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

The site is located within the urban area and there are residential dwellings surrounding the site and the proposal would not therefore conflict with the surrounding land uses. Appropriate infrastructure would be available and the site is located in an accessible location, within 8 minutes walk of Radcliffe town centre. As such, the proposed development would be acceptable in principle, and would be in accordance with Policy H1/2 of the Bury Unitary Development Plan and the NPPF.

Furthermore, as noted above, planning permission for residential development has been approved at the site on two separate occasions, having been considered against the same UDP policies that relate to the present proposal. Whilst the NPPF did not relate to those planning applications, the latest version indicates that planning permission should be granted unless paragraph 11 (d) i or ii apply.

Design and layout

H2/1 - The Layout of New Residential Development and H2/2 - The Layout of New Residential Development, provides the assessment criteria for detailed matters relating to height, appearance, density and character, aspects and finish materials.

The proposed development would provide a mix of detached, semi-detached and terraced dwellings, framing streets which would be laid out to reflect the traditional layout of terraced streets to the east. The proposed dwellings would be of a simple form and character, again to reflect the predominant residential character, although the arrangement would be of a lower density to facilitate vehicle parking and private and communal amenity space. However, the layout would form robust building lines that would result in a clear delineation of streets with framing dwellings and the associated curtilages.

The layout of dwellings fronting onto School Street has been amended (partly to enable the retention of the majority of existing mature trees) to reflect the wider existing traditional residential character and that within the core of the proposed development. The built form would be framed by generous soft landscaping and retained mature trees. Similarly, the dwellings at proposed plots 69-75 (onto Ulundi Street) have been re-orientated to have their principal elevation fronting the existing street (as opposed to backing onto it and presenting a fenced boundary). This would reinforce the layout of the existing street.

The proposed materials would reflect those existing in the wider area with brick facades, which would be render free with grey tiles.

Existing boundary iron railings and former gate posts to School Street/Ulundi Street would be retained where possible. 1.8m brick wall and piers with timber panel infills would frame principal gateways into and within the site and close boarded timber fencing would be used principally to separate gardens.

All of the proposed dwellings would have a side or rear gardens, which would provide a suitable level of private amenity space. There would be space within the side/rear gardens for bin storage.

Given the above, the proposed development would assimilate not be a prominent feature in the streetscene and would be in accordance with Policies EN1/2, H2/1 and H2/2 of the Bury Unitary Development Plan.

Impact upon residential amenity

Neighbouring amenity

SPD6 provides guidance on aspect standards between residential properties and as such, would be relevant in this case. The aspect standards states that there should be a minimum of 20 metres between directly facing habitable windows and 13 metres between a habitable room window and a two storey blank wall.

The separation distances between the proposed dwellings would comply with the aspect standards in SPD6.

Concerning the relationship to existing dwellings surrounding the site, the proposed dwellings at plots 69-75, which have been re-orientated and re-positioned (as noted above), would be approximately 14m from the opposing front elevations of existing dwellings. This arrangement would reflect the existing layout of Ulundi Street to the immediate north and the relationship between opposing dwellings on other nearby terraced streets. Furthermore, the proposed dwellings at plots 69-75 would have small forecourt gardens that would frame the street on the west side and soften the character and appearance of the development. It is considered that the wider benefit of creating a robust and more attractive street scene with dwellings either side fronting it justifies the tolerance of a separation distance that reflects and reinforces the traditional layout.

All other relationships to surrounding dwellings would meet the suggested separation distances.

Objections refer to potential impacts on neighbouring amenity due to construction activities. Such matters can be controlled by agreement of a Construction Traffic Management Plan, to be agreed by condition. The hours of operation fall within the Environmental Pollution Act considerations.

Other objections refer to the impact of the access into the site from School Street and the impact of the associated vehicles by way of noise (queuing on School Street) and head light nuisance to opposing dwellings. Existing dwellings on the south side of School Street front onto an adopted road within the urban area opposing a brownfield site that has had planning permission on two occasions previously (having been approved under the same UDP policies). Furthermore, dwellings in many urban settings oppose junctions and it is not an anomalous juxtaposition. Headlights on cars tend to be in a default 'dipped' position and dwellings typically have blinds or curtains drawn during darker hours.

Similarly, the neighbouring school to the east was approved under the same planning permission as outline permission for residential development at the site under consideration and there is a significant area of landscape buffer and intervening security fencing that would be retained. GM Police have not raised any concerns.

Given the above circumstances, the proposed development would not conflict with the relevant policies.

Occupiers amenity

UDP Policy EN7/2 states that the Council will not permit development close to a permanent source of noise.

Given the relationship of those dwellings at the northern part of the site adjacent to the boundary to the existing builder's merchant, the proposal is supported by a Noise & Building Envelope Assessment, which was undertaken in accordance with BS 4142 - Method for rating and assessing industrial and commercial sound and BS 8233:2014 - Guidance on sound insulation and noise reduction for buildings.

The Assessment concludes that the results of the noise impact indicate that the builder's merchant would have a 'low' impact at the nearest residential receivers, subject to the installation of a 2m high acoustic fence for the full length of the northern boundary and stated construction methods.

The Council's Pollution Control Section is satisfied that subject to the implementation of the recommendations of the Assessment, the proposal would not have unacceptable impacts on occupier's amenity.

The 'Technical housing standards – nationally described space standard (NDSS) (2015)' concerns internal space within new dwellings and is suitable for application across all tenures. It sets out requirements for the Gross Internal (floor) Area of new dwellings at a defined level of occupancy as well as floor areas and dimensions for key parts of the home, notably bedrooms, storage and floor to ceiling height. The requirements of this standard for bedrooms, storage and internal areas are relevant only in determining compliance with this standard in new dwellings and have no other statutory meaning or use.

The standard has not been adopted as a local plan policy (given that Bury UDP dates from 1997), but it represents a useful guide as to what may constitute an acceptable standard of development.

The development would not meet the standards. However, the applicant notes the presence of developments within the vicinity of the site that have been determined and are being built out, or have recently been approved, where space standards have also not been met. In this instance, given that Bury do not currently have a local plan policy that requires development to adhere to NDSS and the tilted balance that needs to be applied when considering paragraph 11(d) of the NPPF, the non-compliance of the dwellings with NDSS is not considered to be so significant that it would warrant refusal of the scheme.

Accessibility, Highways and Parking

UDP Policy EN1/2 requires the consideration of the design and appearance of access, parking and service provision. This is further supported by UDP Policy H2/2 that requires proposals to demonstrate adequate car parking provision, access for both vehicles and pedestrians, and provision for public transport and the existence of any public rights of way, and UDP Policy HT6/2 that requires developments to reduce pedestrian/vehicular conflict. UDP Policy HT2/4 requires all applications for development to make adequate provision for their car parking and servicing requirements. Supplementary Planning Document 11 provides parking standards for developments.

Objections largely refer to highway concerns as noted above.

A Transport Statement has been prepared to accompany the planning application with further addendums in response to matters raised by TfGM and the Council's Highway Officer, who is aware of the nature and content of the objections.

The consultation responses from TfGM and the Council's Highway Officer are reported above. Neither object to the proposal and recommend that the proposal is acceptable, subject to the conditions that would require agreement of the re-positioning of speed cushions and a review of waiting restrictions to ensure that access and egress into the site and the local highway network operates in a safe manner. Further conditions are noted above, which would include a requirement to make good any residual damage to the surrounding highway network caused by construction activity.

In relation to the TfGM recommendation for a contribution for the improvement of pedestrian facilities at the junction of Pilkington Way / Sion Street / Dale Street, the applicant has submitted an assessment of the pedestrian routes from the site to facilities in and around Radcliffe town centre. This finds that there are other, more likely pedestrian routes into the town centre (than via Pilkington Way/Sion Street/Dale Street) that are more direct, well lit and safer, better surfaced, and some of which will be improved as part of City Region Sustainable Transport Settlement (particularly around Green Street/May Street/Railway Street). This would strengthen the applicant's assumption that School Street/Blackburn Street or pedestrian footbridge (via Green Street) would represent the most desirable and the shortest pedestrian routes to the town centre from the site.

The assessment concludes that given that other more attractive (more pedestrian friendly) routes are available to residents, and whilst some residents may choose to use the Pilkington Way/Sion Street/Dale Street junction, the numbers are likely to be low and therefore a contribution would not be proportional to the size and scale of the development.

Given the circumstances set out by the applicant, it is not considered that it would be either proportionate or justified to seek the off-site mitigation works suggested by TfGM.

Each dwelling would have 2 off-street parking spaces, provided either to the front or to the side of each property and this is considered adequate given the high accessibility of the site.

Ecology and landscaping

The objection refers to the loss of trees at the site. However, the proposal has been amended since the original submission and the two principal groups of trees would be retained in the main.

A Preliminary Ecological Appraisal has been submitted.

The Greater Manchester Ecology Unit (GMEU) have raised no objections to the proposal, subject to a condition to protect bats.

Section 174 of the NPPF 2021 states that the planning policies and decisions should contribute to and enhance the natural and local environment. The site currently comprises mixed scrub, neutral grasslands, base ground and urban trees.

The proposed development would replace this with low value ecological value habitats and bare ground. The ecological report notes that only partial compensation would be possible on site and off-site compensation would be required. The GMEU consider that the latest iteration of the supporting Biodiversity Net Gain Assessment (following amendment to the proposed layout and retention of the most significant existing trees) significantly over-estimates the value of the existing habitat (earlier assess methods gave higher values to urban trees). They have therefore advised that the habitat loss (after the introduction of a landscaping scheme) would equate to 2.32 biodiversity units.

Given that the biodiversity baseline was established in 2021 in relation to planning permission 66307, this translates into a contribution of £23,200 (i.e. £10,000 x 2.32).

Therefore, subject to conditions and a contribution for off-site biodiversity mitigation, the proposed development would not cause harm to a protected species and would be in accordance with Policies EN6 and EN6/3 of the Bury Unitary Development Plan and the NPPF.

Trees and landscaping

An Arboricultural Constraints Appraisal has been submitted in support of the application and the proposal has been amended to retain a significant level of existing tree cover.

The proposal is also supported by a comprehensive soft landscaping scheme, which includes, as mitigation for the loss of trees, the provision for the planting of a mixture of native as well as ornamental trees, shrubs and hedges and tree protection measures.

GMEU are satisfied that the proposed replacement landscaping (in tandem with the off-site mitigation noted above) would be satisfactory.

Therefore, the proposed development, subject to conditional control, would not harm the character of the area and would be in accordance with Policy EN8/2 of the Bury Unitary Development Plan.

Other matters

Greater Manchester Fire & Rescue Service (GMFRS)

The applicant has confirmed that the proposal would be in accordance with the technical requirements set out in the consultation response from the GMFRS.

Ground conditions

The Environmental Protection Officer and the Coal Authority do not object to the proposal, subject to the above noted conditions.

Air quality

The Environmental Protection Officer has no objection to the proposal, subject to the inclusion of a condition relating to the installation of electric vehicle charging points.

Drainage

United Utilities do not object to the proposal, subject to conditions relating to a surface and foul water drainage scheme.

Waste Management

Each proposed dwelling would be adequately provided for the storage of waste and recycling facilities with collection point pertaining to unadopted streets.

Planning obligations

Recreation - A contribution of £246,319.40 would be required towards recreation in accordance with Policy RT2/2 of the Bury Unitary Development Plan and SPD1.

Affordable Housing - The proposed development would provide 22 affordable units, which would equate to 25% of the total number of units in accordance with Policy H4/1 of the Bury Unitary Development Plan and SPG5.

Of the 22 affordable units, 6 (25%) would be provided as First Homes at a 30% discount.

The affordable units would be adequately pepper-potted around the site and as such, would comply with the policy requirement for pepper potting throughout.

Off-site biodiversity mitigation - As noted above, the proposal generates a requirement to replace 2.32 biodiversity units, translating into a contribution of £23,200, which would be allocated to a site known as Walker's Field, Chesham, in accordance with paragraph 174 of the NPPF.

Observation on representations received

Many of the points raised in the representations, where material, have been addressed in the above analysis and/or recommended conditions.

On the specific point relating to claimed inaccuracies, the local planning authority is satisfied that the submitted plans and supporting information has enabled a robust assessment of the proposal and the potential impacts.

CONCLUSION

Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that if regard is to be had to the Development Plan for the purpose of any determination to be made under the Planning Acts, the determination must be in accordance with the plan, unless material considerations indicate otherwise.

The 'titled balance' applies and the NPPF therefore requires that planning permission should be granted any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework, taken as a whole.

Given the status of the site, the principle of the development is acceptable. The proposal would deliver much needed residential development on a previously developed site within

the urban area that is considered to be in a highly sustainable location. It would provide a mix of good quality market and affordable dwellings in a layout that would create varied and attractive streets, framed in a wider context of robust and extensive landscaping and open space.

Further, the development would not unacceptably undermine residential amenity, highway safety or ecological interests and would provide a contribution for off-site biodiversity mitigation.

Other constraints including drainage and flooding, contamination, coal mining risk and air quality impacts have been adequately addressed.

On this basis, the scheme, along with the legal agreement to secure the above noted contributions, the development would comply with the Unitary Development Plan and the NPPF when considered as a whole.

As such, the proposal would comply with the above policies of the Local Plan and the NPPF. Therefore, in accordance with Section 38(6) of the Planning and Compulsory Purchase Act, the proposal merits approval.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

Recommendation: Minded to Approve

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. Subject to the requirements of the conditions below, the development hereby permitted shall be carried out in accordance with the following approved drawings and documents:
Drawings:
Location plan (dwg no. SL01)
External Works - Sheet 1 (Dwg No. 21196-EDGE-XX-XX-DR-C-1101 P02)
External Works - Sheet 2 (Dwg No. 21196-EDGE-XX-XX-DR-1102 P02)
Plot Drainage - Sheet 2 (Dwg No. 21196-EDGE-XX-XX-DR-C-2012 P02)
External Works - Sheet 1 (Dwg No. 21196-EDGE-XX-XX-DR-C-1101 P02)
1.8m Brick Wall with Piers with Timber Infill Panels (Dwg No. BW01)
1800mm High Feathered Access Door (Dwg No. TG01)
1.8m Feather Edges Timber Fence (Dwg No. FB01)
Newbold End - Floor Plans (Dwg No. NB-END-001 A)
Newbold End - Elevations (Dwg No. NB-END-002 A)
Bowker mid - elevations and floor plans (Dwg No. BO-MID-001 18-122 REV.C)
Brooklands house type - elevations and floor plans (Dwg No. BR-001 18-122 REV.B)
Construction details (Dwg no. 21196-EDGE-XX-XX-DR-C-3201 -P01)

Bowker Alternative House Type - floor plans and elevations (Dwg no. BO-ALT-001)
Timber fence detail (Dwg no. FB01)
Hollinwood House Type - proposed floor plans and elevations (Dwg no. Hw 001)
Timber gate detail (dwg no. TG01)
Brooklands Alternative Bay House Type - proposed elevations and floor plans (Dwg no. BR ALT B-001)
Cornbrook Alt House Type - proposed floor plans and elevations (Dwg no. CO-ALT-001)
Cornbrook end House Type - proposed floor plans and elevations (Dwg no. CO-END-001)
Cornbrook mid house type - proposed floor plans and elevations (Dwg no. CO-MID-001)
Firswood house type - proposed floor plans and elevations (Dwg no. FI-001)
Newhey Alternative Bay House Type - proposed floor plans and elevations (Dwg no. NH-ALT B-001)

Proposed Layout (Colour) (Dwg No. 21-039 PL01 U)
Proposed Layout (Dwg No. 21-039 PL01 U)
Catchment Analysis ((Dwg No. 21196-EDGE-XX-XX-DR-C-2004 Rev. P03)
Manhole schedule (dwg no. 21196-EDGE-XX-XX-DR-C-2201 P03)
Longitudinal Sections - Sheet 2 (dwg no. 21196-EDGE-XX-XX-DR-C-2102 P03)
Road Setting Out - Sheet 1 (dwg no. 21196-EDGE-XX-XX-DR-C-3002 P02)
Road Setting Out - Sheet 2 (dwg no. 21196-EDGE-XX-XX-DR-C-3003 P02)
Kerb and Surfacing Layout (dwg no. 21196-EDGE-XX-XX-DR-C-3006 P02)
Longitudinal Sections - Sheet 1 (dwg no. 21196-EDGE-XX-XX-DR-C-2101 P03)
Drainage Layout (dwg no. 21196-EDGE-XX-XX-DR-C-2002 Rev P03)
Bowker End (Dwg No. BO-END-001 G)
Indicative Sections (Dwg No. 21-039 CS01 C)
Hard Landscaping Plan (Dwg No. 21-039 HL01 B)
Land Use Plan (Dwg No. 21-039 LU01 B)
Boundary Details Plan (Dwg No. 21-039 BD01 G)
Indicative Streetscenes (Dwg No. 21-039 SS01 B)
Proposed Soft Landscaping Plan - Sheet 1 of 2 (Dwg No. 21-039 L-S-001 B)
Proposed Soft Landscaping Plan - Sheet 2 of 2 (Dwg No. 21-039 L-S-002 B)
Materials Plan (Dwg No. 21-039 MP01 B)
Refuse Management Plan (Dwg No. 21-039 RM01 B)

Documents:

Addendum Gas Risk Assessment and Updated Remediation Strategy (Ref. LKC 20 1836)
Ambient Noise and Building Envelope Assessment (Ref. J004296-6055-TD-01)
Crime Impact Statement (2022/0362/CIS/01)

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

3. No development shall commence unless and until:-
- A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory

development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

5. No development comprising the erection of any external walls shall take place until details of the materials to be used in the construction of the external surfaces of the development, including the roof, and all boundary treatments, have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details. The materials to be used throughout the development shall be consistent in terms of colour, size and texture with the approved details.

Reason. In the interests of visual amenity and to ensure a satisfactory development pursuant to Policy EN1/2 - Townscape and Built Design of Bury Unitary Development Plan.

6. Each dwelling hereby approved shall be provided with 1 electric vehicle (EV) charge point (minimum 7kW*) prior to its occupation.

*Mode 3, 7kW (32A) single phase, or 22kW (32A) three phase, and for 50kW Mode 4 rapid charging may be required. British Standard BS EN 61851-1:2019 to be used.

Reason. To encourage the uptake of ultra-low emission vehicles and ensure the development is sustainable. To safeguard residential amenity, public health and quality of life, pursuant to Section 9 of the National Planning Policy Framework.

7. Notwithstanding the details indicated on the approved plans and in the supporting information, no development shall commence unless and until full details of the following have been submitted to a scope and specification to be agreed on a topographical based survey of the site and adjacent adopted highways to the Local Planning Authority:

1. Formation of the accesses onto School Street, incorporating the provision of adequate arrangements at the interface with the adopted highway and 1 in 20 plateau (maximum) at the junction, relocation of the affected speed cushions in a position to be agreed and any associated accommodation works, level access to the private footpaths serving Plots 80 - 85 and the public open space between Plots 79 & 80, provision of visibility splays appropriate for a design speed of 20mph, demarcation of the limits of the adopted highway, tactile paved crossing points, give-way markings and any associated alterations to existing road markings (subject to the requirements of the scheme of 20mph traffic calming measures and Traffic Regulation Order review) and all associated highway and highway drainage remedial works;

2. Formation of the access onto Abden Street/un-named back street adjacent to No.'s 65/67 Ulundi Street, incorporating the provision of adequate arrangements adequate arrangements at the interface with the adopted highway and tying-in with the existing kerblines and parking lay-by/road markings on Abden Street, 1 in 20 (maximum) plateau at the junction, reconstruction of the section of Abden Street between Ulundi Street and the easterly site boundary and scheme of road markings/coloured surfacing,

- alterations to the boundaries at Plots 43/43 to form an acceptable junction with the back street and provide visibility splays appropriate for a design speed of 20mph, demarcation of the limits of the adopted highway, tactile paved crossing points, give-way markings and alterations to existing road markings (subject to the requirements of the scheme of 20mph traffic calming measures) and all associated highway and highway drainage remedial works;
3. Revised footpath arrangements in the vicinity of Plot 69 to ensure that the new pedestrian route connects to and at the correct level on the existing southerly footway on Chard Street and all associated accommodation works on the adopted highway;
 4. All works around the perimeter of the site to either retain or replace the existing school boundary walls, pillars and railings, incorporating, where required, new foundations that do not encroach under the adopted highway, demarcation of the limits of the adopted highway with flush/50mm upstand edgings (as appropriate) and all associated accommodation works on the adopted highway;
 5. In the event that the site access/estate roads are not to be considered for adoption by the Council, measures to prevent the discharge of surface water onto the adopted highway;
 6. In the event that any retaining structures are required that abut/are sited in close proximity to the adopted highway, an 'Approval In Principle' for the proposed structures, incorporating full structural, construction and drainage details, calculations, pedestrian protection measures and a detailed construction method statement;
 7. Provision of a street lighting assessment to a scope and specification to be agreed for School Street between the site access and Hutchinson Way, Abden Street between the site access and Blackburn Street, the proposed residential estate roads, and, where necessary, a scheme of improvements;
 8. Review of need for the introduction of waiting restrictions to scope to be agreed at the junction of the site accesses with School Street and Abden Street, including, if required, all necessary road markings and signage;
 9. A scheme of 20mph traffic calming measures on the proposed estate roads to a scope and specification to be agreed, incorporating the formation of speed tables at all appropriate junctions, road markings, signage and works within the existing as required.

The details subsequently approved shall be implemented in full to a programme agreed with the Local Planning Authority.

Reason. To secure the satisfactory development of the site in terms of highway safety, ensure good highway design, in the interests of fire safety, ensure the intervisibility of the users of the site and the adjacent highways and maintain the integrity of the adopted highway, all in the interests of highway safety, pursuant to policies H2/2, EN1/2 and HT6/2 of the Bury Unitary Development Plan.

8. In the event that it is intended for the proposed residential estate roads to be considered for adoption by the Council, notwithstanding the details indicated on the approved plans and supporting information, no development shall commence unless and until full details of the following have been submitted on a topographical based survey of the site and adjacent adopted highways to the Local Planning Authority:
 1. Formation of the proposed estate roads (including any necessary land filling operations and method of compaction), incorporating minimum 5.5m carriageway and 2.0m footway widths;
 2. Provision of long sections and cross sections at positions to be agreed through the proposed estate roads and turning heads to ensure that adoptable gradients will not exceed 1 in 14 for the purposes of adoption and incorporating a maximum 1 in 20 plateau at each junction within the development and at the interface with the adopted highway;
 3. Demarcation of the limits of adoption at all relevant locations;

4. Provision of visibility splays and forward visibility envelopes appropriate for a design speed of 20mph at all internal junctions and bends in accordance with the standards in Manual for Streets with no obstructions above the height of 0.6m within them;
5. Revised access/parking arrangements for Plots 5, 31 & 36 to ensure that the new dwellings are served from the adjacent private shared accesses and to mitigate the need for long reverse manoeuvres into the adjacent junctions along the proposed adopted footways that would otherwise result from the layout as submitted;
6. Revised access/parking arrangements for Plot 40 to avoid the proposed driveway straddling the anticipated limits of adoption;
7. Provision of adequate levels of visibility at the back edge of the footway at Plots 27, 38 & 39;
8. Swept path analysis of the proposed estate roads to ensure that an 11.85m long refuse collection vehicle can pass a private car at all points and manoeuvre at all turning heads;
9. Facilities for the storage of bins on collection day at the interfaces of all shared accesses with the proposed adopted highway;
10. Proposed planting/surfacing details and management/maintenance arrangements for all landscaped/car parking areas abutting the proposed adopted highway;
11. Proposed porous/permeable shared driveway/hardstanding materials and/or measures to prevent the discharge of surface water onto the adopted highway.

The details subsequently approved shall be implemented to an agreed programme and to the written satisfaction of the Local Planning Authority.

Reason. To secure the satisfactory development of the site in terms of highway safety, ensure good highway design, ensure the intervisibility of the users of the site and the adjacent highways and maintain the integrity of the adopted highway, all in the interests of highway safety. pursuant to policies H2/2, EN1/2 and HT6/2 of the Bury Unitary Development Plan.

9. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and agreed in writing with the Local Planning Authority and shall confirm/provide the following:
 1. Photographic dilapidation survey of the footways and carriageways leading to and abutting the site in the event that subsequent remedial works are required following construction of the development and as a result of statutory undertakers connections to the site;
 2. Access route for all construction vehicles to the site from the Key Route Network, including the need to prohibit/limit the use of the residential street in the Abden Street/Lord Street area;
 3. Access point(s) for construction traffic taking into consideration existing residential properties and their accesses onto School Street and any temporary works/phasing of the development that may be required to facilitate safe access for construction vehicles;
 4. If proposed, details of site hoarding/gate positions, taking into consideration the need to maintain adequate levels of visibility onto the adopted highway;
 5. The provision, where necessary, of temporary pedestrian facilities/protection measures on the highway;
 6. A scheme of appropriate warning/construction traffic speed signage in the vicinity of the site and its access(es);
 7. Confirmation of hours of operation and number of vehicle movements;
 8. Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site and/or measures to control/manage delivery vehicle manoeuvres;
 9. Parking on site or on land within the applicant's control of operatives' and construction vehicles, together with storage on site of construction materials;
 10. Measures to ensure that all mud and other loose materials are not spread onto the adjacent adopted highways as a result of the groundworks operations or

carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations.

The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction period.

Reason. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent highways, ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations., in the interests of highway safety, pursuant to policies H2/2, EN1/2 and HT6/2 of the Bury Unitary Development Plan.

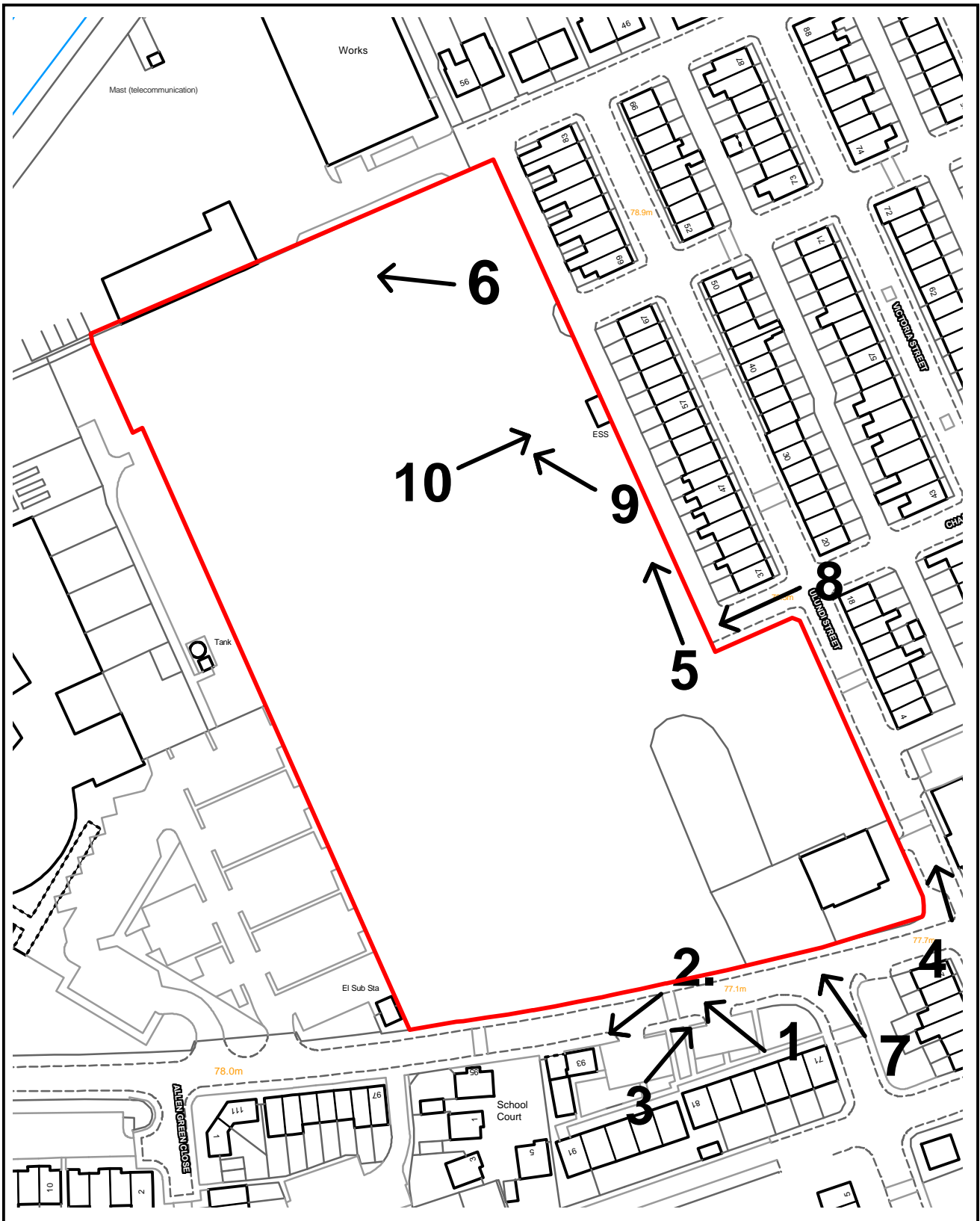
10. In the event that it is not intended for the proposed residential estate roads to be considered for adoption by the Council, no development shall commence unless and until details of the proposed arrangements for future management and maintenance of the proposed estate road within the development have been submitted to and approved by the local planning authority. The estate roads shall thereafter be maintained in accordance with the approved management and maintenance details until such time as a private management and maintenance company has been established.
Reason. This is required prior to the commencement of development to ensure that the unadopted estate roads serving the development is maintained to an acceptable standard in the interest of residential/highway safety, to ensure a satisfactory appearance to the highways infrastructure serving the development, and to safeguard the visual amenities of the locality and users of the highway pursuant to policies H2/2 and EN1/2 of the Bury Unitary Development Plan.
11. The various turning facilities indicated on the approved plans shall be provided before the areas of the development to which they relate are first occupied and the areas used for the manoeuvring of vehicles shall subsequently be maintained free of obstruction at all times.
Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety, pursuant to policies H2/2, EN1/2 and HT6/2 of the Bury Unitary Development Plan.
12. Minimum hardstanding lengths of 5.0m at all dwellings (10.0m when in tandem) shall be provided to the written satisfaction of the Local Planning Authority and thereafter maintained.
Reason. To allow adequate space to maintain vehicles clear of the highway in the interests of pedestrian safety, pursuant to policies H2/2, EN1/2 and HT6/2 of the Bury Unitary Development Plan.
13. The car parking indicated on the approved plans shall be surfaced and made available for use to the satisfaction of the Local Planning Authority prior to the each dwelling/part of the site to which it relates being occupied and thereafter maintained at all times.
Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policy HT2/4 of the Bury Unitary Development Plan.
14. Bin storage shall be provided within the curtilage of each dwelling in accordance with the approved Refuse Management Plan (Dwg No. 21-039 RM01 B).
Reason. To ensure that adequate bin storage arrangements are provided within the curtilage of each dwelling, pursuant to policies H2/2 and EN1/2 of the Bury Unitary Development Plan.
15. Any tree assessed as low risk by either TEP in the preliminary ecological assessment dated February 2021 drawing number g8603.001, or Arbtech in the preliminary ecological appendix 4 Habitat survey plan, shall be removed using soft felling techniques under the guidance of an ecological consultant, unless

otherwise agreed in writing by the Local Planning Authority.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.

16. The drainage for the development hereby approved shall be carried out in accordance with the principles set out in the approved Drainage Layout 211196-EDGE-XX-XX-DR-C-2002 Rev P03, dated 11/05/2023, which was prepared by EDGE. For the avoidance of doubt surface water must drain at the restricted rate of 22.3l/s. Prior to occupation of the first dwellinghouse, the drainage scheme shall be completed in accordance with the approved details and retained thereafter for the lifetime of the development.
Reason. To ensure a satisfactory form of development and to prevent an undue increase in surface water run-off and to reduce the risk of flooding, pursuant to Policy EN5/1 of the Bury Unitary Development Plan and Section 14 of the National Planning Policy Framework.
17. All trees to be retained on site, as indicated on approved drawing Proposed layout 21-039 PL01 Q, shall be protected in accordance with BS 5837:2012 "Trees in relation to design, demolition and construction". The development shall not commence unless and until the measures required by the British Standard are implemented and all measures required shall remain in situ until the development has been completed.
Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.
18. The hard and soft landscaping shall be carried out in accordance with the approved Proposed Soft Landscaping Plan - Sheet 1 of 2 (Dwg No. 21-039 L-S-001 B) and Proposed Soft Landscaping Plan - Sheet 2 of 2 (Dwg No. 21-039 L-S-002 B). All planting, seeding and/or turfing shall be carried out in the first planting and seeding seasons following the occupation of the building(s) relating to that phase or the completion of the development, whichever is the sooner, and any trees, shrubs, hedges or plants which within a period of five years from the completion of development die, are removed, or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species, unless the Local Planning Authority gives written consent to any variation.
Reason. To secure the satisfactory development of the site and in the interests of visual amenity and to ensure the protection of wildlife and supporting habitat and to secure opportunities for enhancing the site's nature conservation value, pursuant to policies EN6/4, EN8/2 and EN10/2 of the Bury Unitary Development Plan and Section 15 of the National Planning Policy Framework.
19. The acoustic fence to the northern boundary of the site, as detailed in the submitted Ambient Noise & Building Envelope Assessment (Report Ref. J004296-6055-TD-01), shall be implemented in full prior to the occupation of the first dwellinghouse and where building fabric mitigation measures are indicated, these shall be completed in full prior to the first occupation of the associated dwellinghouse.
Reason. In order to ensure that the amenities of the future occupants of the proposed properties are not adversely impacted by noise from the nearby industrial builder's merchant, in accordance with Policy EN7/2 of the Bury Unitary Development Plan and the Section 15 of the National Planning Policy Framework.

Viewpoints - 69233



PLANNING APPLICATION LOCATION PLAN

APP. NO 69233

ADDRESS: Site of the former Riverside High School,
School Street, Radcliffe, M26 3BW

Planning, Environmental and Regulatory Services

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69233

Aerial



Existing character



69233

Photo 1: School Street frontage



Photo 2 - Newer suburban housing opposite site



69233

Photo 3 -Opposite direction



Photo 4 - North along Ulundi Street (from junction with School Street)



69233

Photo 5 Rear of houses at eastern boundary



Photo 6 - Millwood School (left) and builders merchants (right)



69233

Photo 7 - Panorama from School Street



Photo 8 - Panorama from Ulundi Street



Photo 9 - Panorama west/north



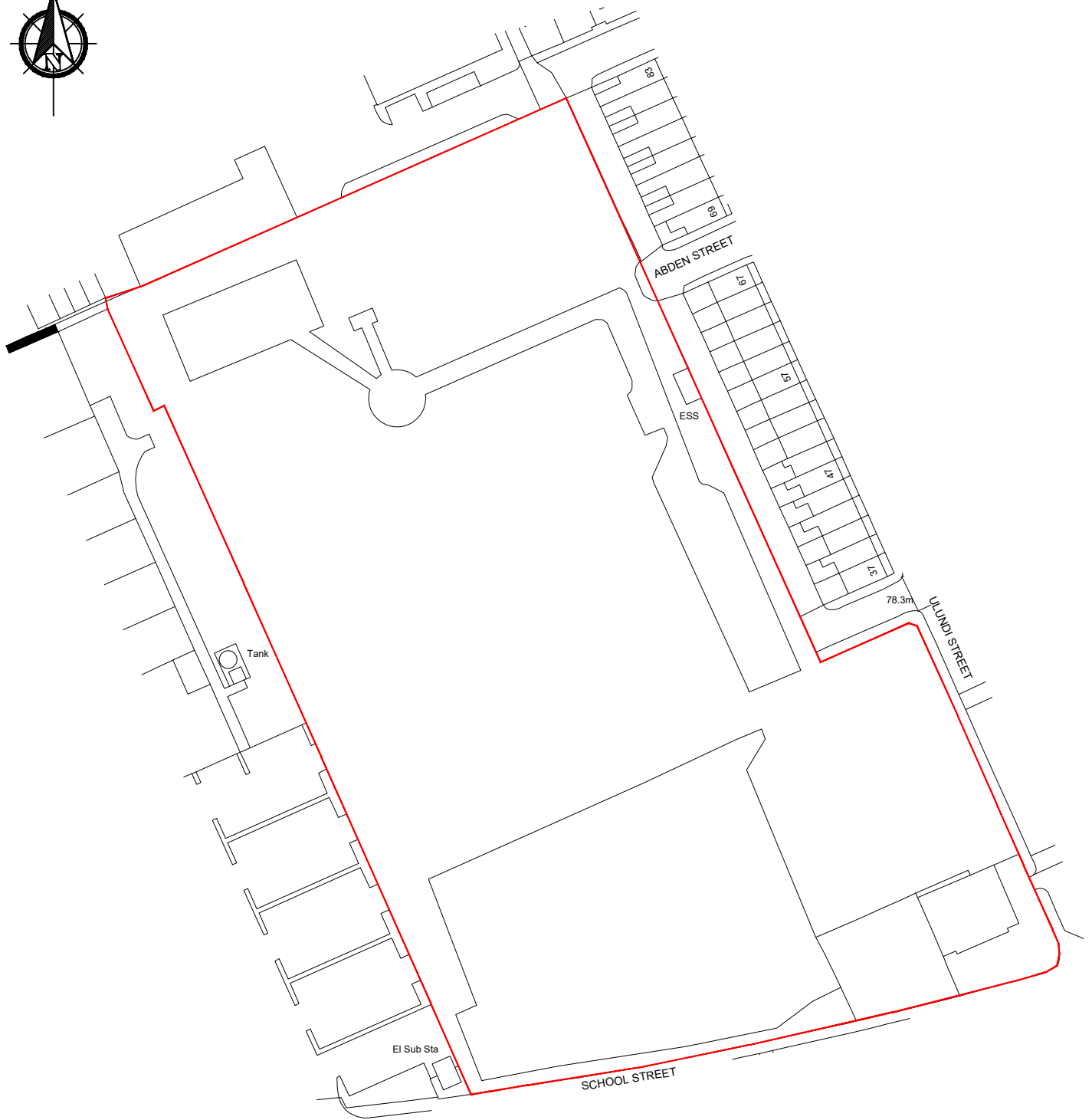
Photo 10 - Panoramas east



69233

Photo 11 - Illustrative Street Scene (School Street)





PLANNING

Client:
HIVE HOMES

Project:
SCHOOL STREET, RADCLIFFE

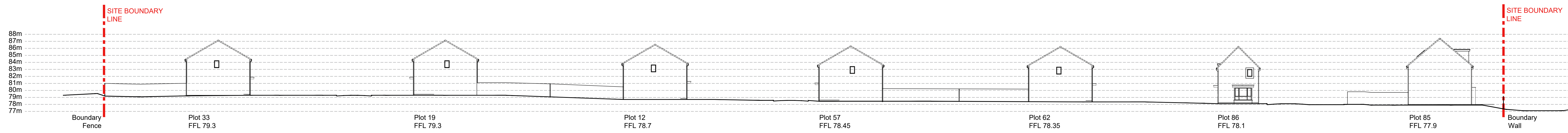
Drawing Title:
SITE LOCATION PLAN

architecture | building surveying | urban design

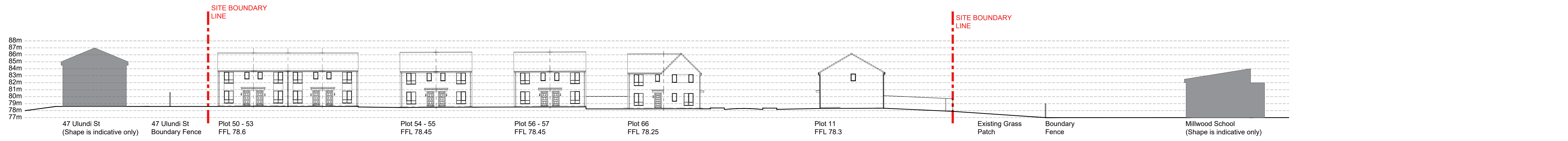
burnaby villa ■ 48 watling street road ■ fulwood ■ preston ■ pr2 8bp
tel: 01772 774510 fax: 01772 774511 email mck@mckassociates.co.uk

Drawn: W.L	Checked:	Scale: 1:1250	Date: JULY 2021	Job No: 21-039	Drawing No: SL01	Rev:
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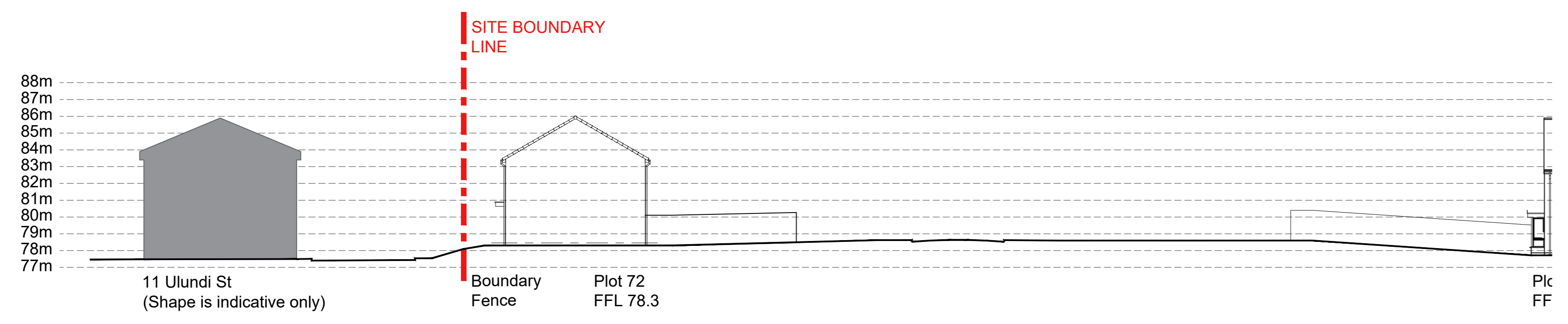
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SECTION A-A



SECTION B-B



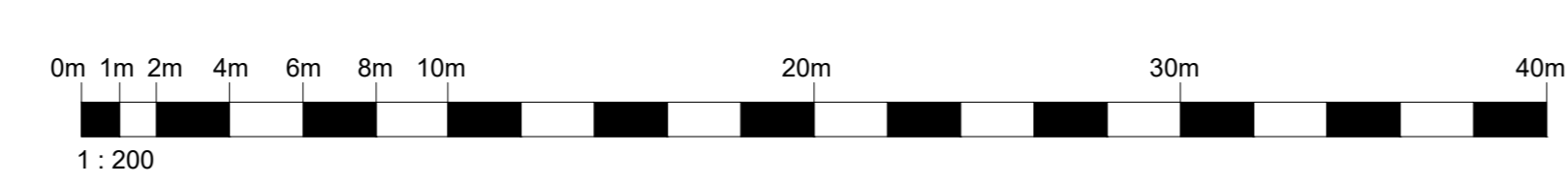
SECTION C-C



NOTE:
 PLEASE NOTE, SECTIONS
 ARE INDICATIVE ONLY

SUBJECT TO FINAL
 ENGINEER FINISHED FLOOR
 LEVELS

PLEASE NOTE,
 TOPOGRAPHY LEVELS
 OUTSIDE OF RED EDGE ARE
 INDICATIVE DUE TO
 INACCESSIBILITY DURING
 SURVEYS



"rights reserved. Licence number 10002243"

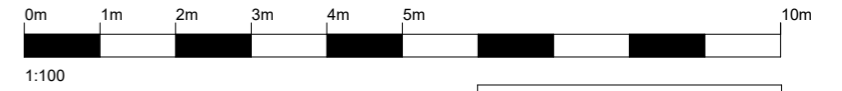
C	14/07/2023	MR	Shayne McKeown
B	26/06/2023	MR	Shayne McKeown
A	19/07/2023	MR	Dr Hugh Lamb (address only) & Dr Henry (only Name)
Rev	Date	By	Description
Client:			

mck associates limited
 architects • building surveying • urban design
 burnaby vancouver • 48 waiting street road • tulwood • preston • p2 Ebp
 tel: 01772 774510 fax: 01772 774511 email: mck@mckassociates.co.uk

SCHOOL STREET
 RADCLIFFE

Drawing Title: INDICATIVE SECTIONS			
Client:	Drawn by:	Scale:	Rev:
MHR	UT	1:200	15/01/2023
Job No:	Drawn by/No:	Rev:	
21-039	CS01	C	

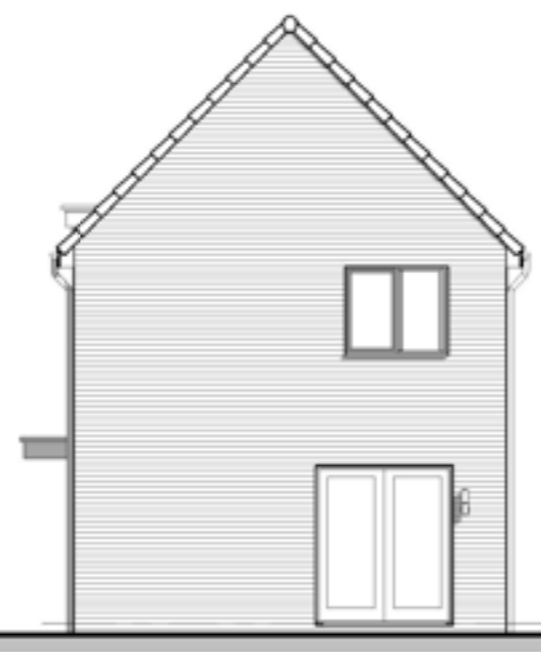
LAYOUT NOT TO SCALE
 -FOR SECTION LINE LOCATION INDICATION ONLY



AREAS		
FLOOR	AREA m ²	AREA ft ²
GROUND	44.4 m ²	478 ft ²
FIRST	42.9 m ²	461 ft ²
TOTAL	87.3 m ²	939 ft ²



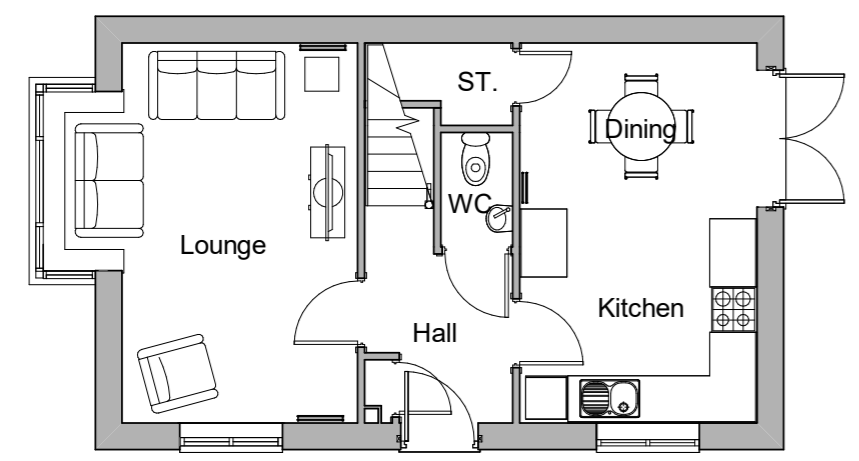
Front Elevation



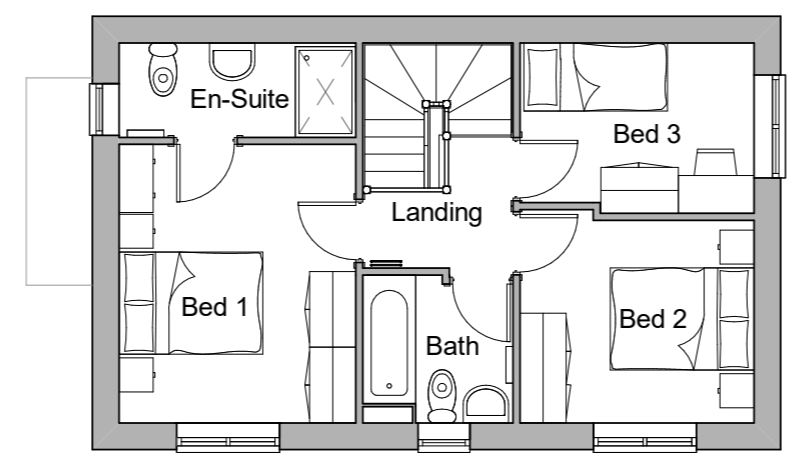
Side Elevation 1



Rear Elevation



Ground Floor





First Floor

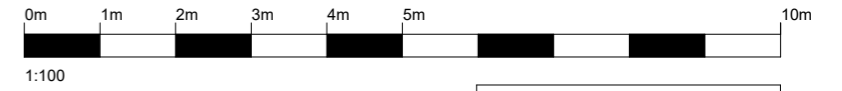


Side Elevation 2

Rev.	Date	by	Description
B	20/10/20	MS	External lighting added
A	6/3/20	DH/HR	Layout amended

Z:\Jobs\2019\18-122 Great Places - House Type Range\Drawings\modelling\proposed\plans\Working Drawings\NH-Alt B.rvt

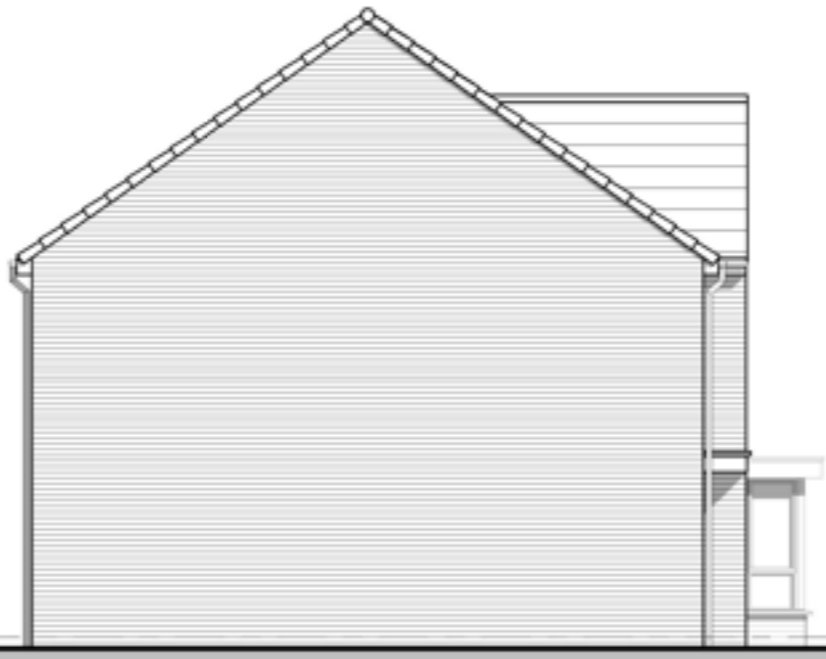
Client: 	Project: Core House Types	Drawing Title: Newhey Alternative Bay 939 Sqft. Code - Nh Alt B	Drawn: MHR Checked: SK	Scale (A3) : 1 : 100 Job No: 18-122	Date: 6/3/2020 Rev: B	<p>General notes: Do not scale the drawing. All dimensions to be checked on site prior to commencement of work and any discrepancy shall be immediately reported and resolved prior to work commencing, this drawing is to be read in conjunction with all relevant drawings and specifications relating to the job whether or not indicated on the drawing. copyright reserved to mck associates ltd. and this drawing may not be used or reproduced without prior written consent.</p>	
PLANNING							



AREA SCHEDULE		
FLOOR	AREA m ²	AREA ft ²
GROUND	52 m ²	564 ft ²
FIRST	51 m ²	548 ft ²
TOTAL	103 m ²	1112 ft ²



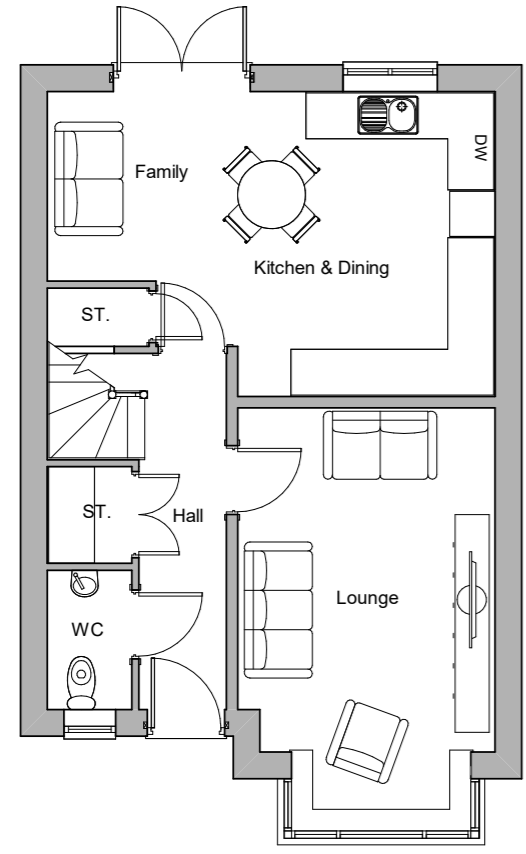
Front Elevation



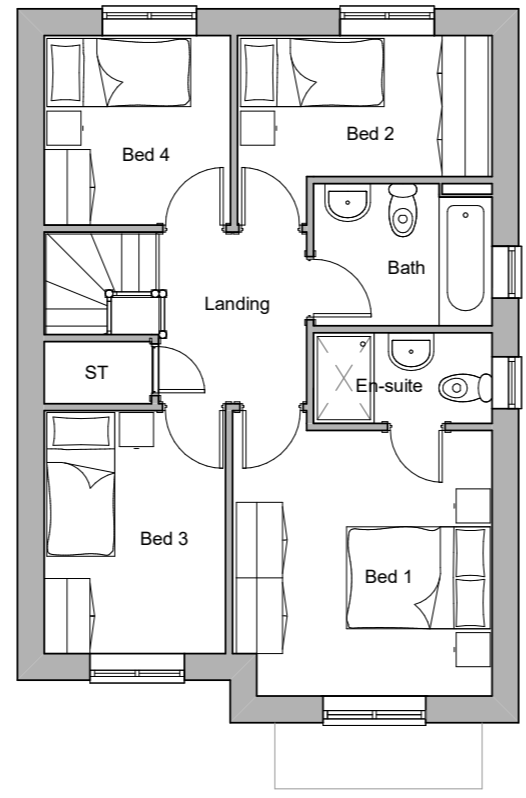
Side Elevation 1



Rear Elevation



Ground Floor Plan



First Floor Plan



Side Elevation 2

D	21/10/20	MS	Elevations amended
C	02/06/20	DH	Minor aesthetic changes
B	21/04/20	GO	kitchen unit amended
A	05/03/20	DH/HR	Layout altered
Rev.	Date	by	Description

Z:\003\HIVE\HIVE Homes - Master Drawing Pack - June 2021\FI-1 - (Fiswood)\FI.rvt



Client: **Project:**
Core House Types

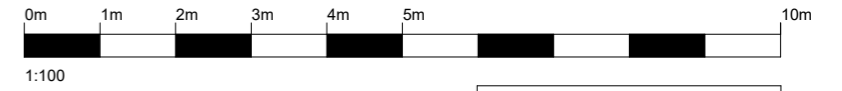
Drawing Title:
**Firwood
1112 Sqft.
Code - Fi**

Drawn: DSH	Checked: SK	Scale (A3) : 1 : 100	Date: 26/11/2019
Drawing No: FI-001		Job No: 18-122	Rev: D

PLANNING

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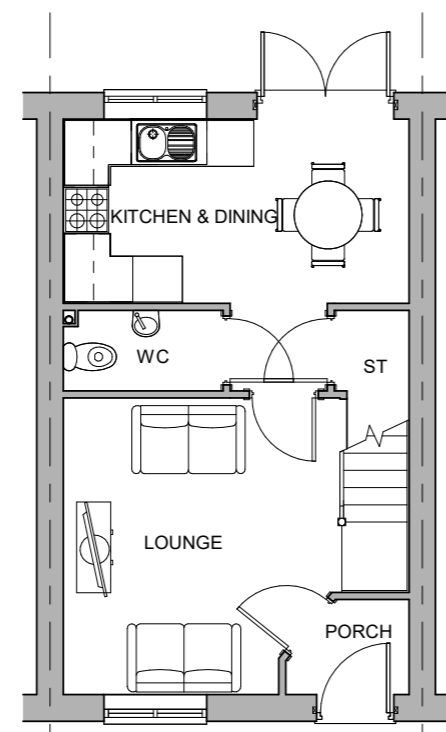
AREA SCHEDULE		
FLOOR	AREA m ²	AREA ft ²
GROUND	35.41 m ²	381 ft ²
FIRST	35.41 m ²	381 ft ²
TOTAL	70.82 m ²	762 ft ²



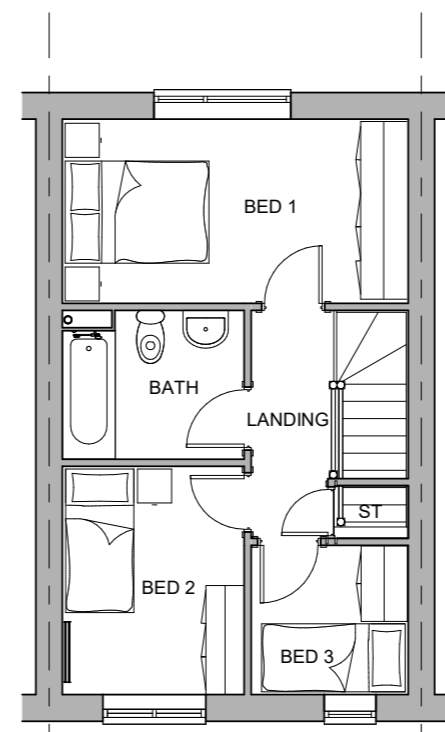
Front Elevation



Rear Elevation



Ground Floor



First Floor

Rev.	Date	by	Description
C	30/04/2020	GO	Kitchen unit amendment
B	30/04/2020	DH/HR	Layout amendments
A	30/04/2020	DH	Canopy updated

Z:\003\HIVE\HIVE Homes - Master Drawing Pack - June 2021\CO MID - (Cornbrook Mid)\Co-Mid.rvt



Client: **Core House Types**

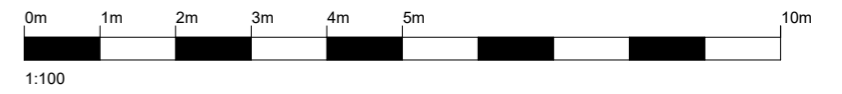
Drawing Title:
Cornbrook Mid
762 Sqft.
Code - Co Mid

Drawn: DH	Checked: SK	Scale (A3) : 1 : 100	Date: 13/06/19
Drawing No: CO-MID-001		Job No: 18-122	Rev: C

PLANNING

General notes:
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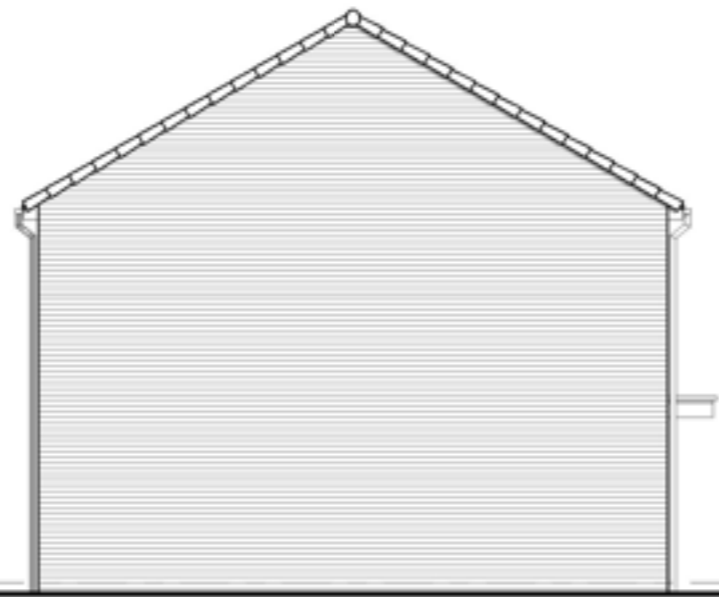




AREA SCHEDULE		
FLOOR	AREA m ²	AREA ft ²
GROUND	35.41 m ²	381 ft ²
FIRST	35.41 m ²	381 ft ²
TOTAL	70.82 m ²	762 ft ²



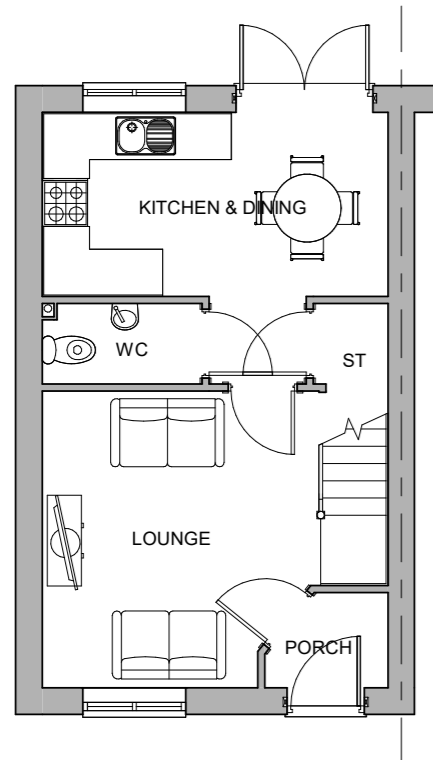
Front Elevation



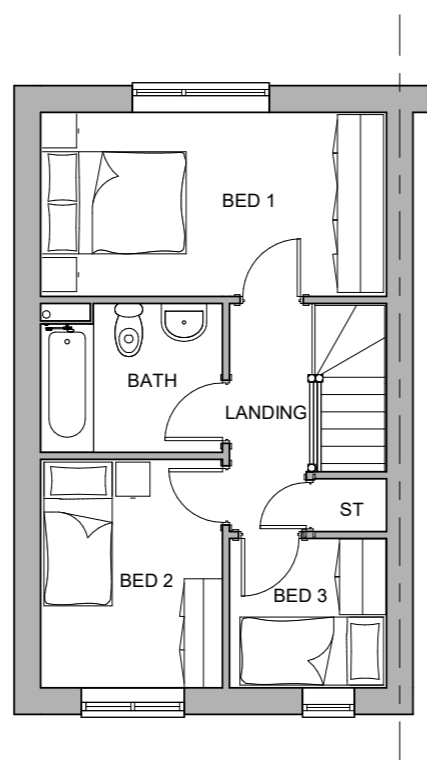
Side Elevation



Rear Elevation



Ground Floor



First Floor

Rev.	Date	by	Description
E	28/10/20	MHR	Layout Updated
D	10/06/20	DH	External light shown
C	03/04/20	HR	Amended the position of window in Bed 3
B	05/03/20	DH/SK	Layouts amended
A	27/02/20	DH	Canopy altered

Client:



Project:
Core House Types

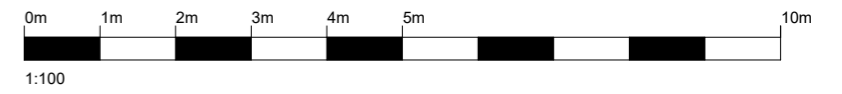
Drawing Title:
Cornbrook End
762 Sqft.
Code - Co End

Drawn: DH	Checked: SK	Scale (A3) : 1 : 100	Date: 13/06/19
Drawing No: CO-END-001		Job No: 18-122	Rev: E

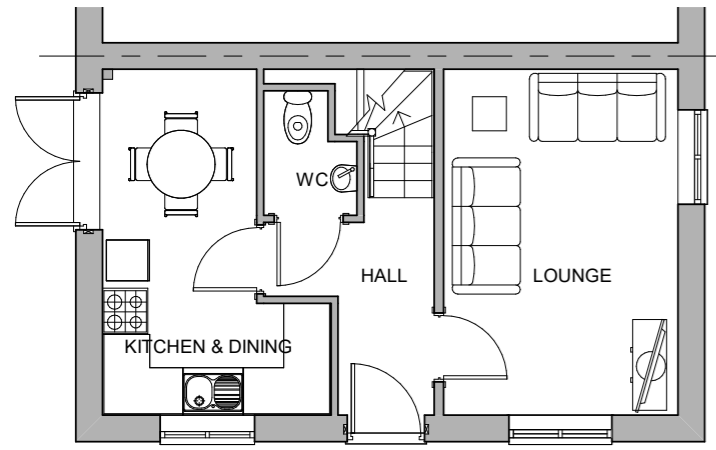
PLANNING

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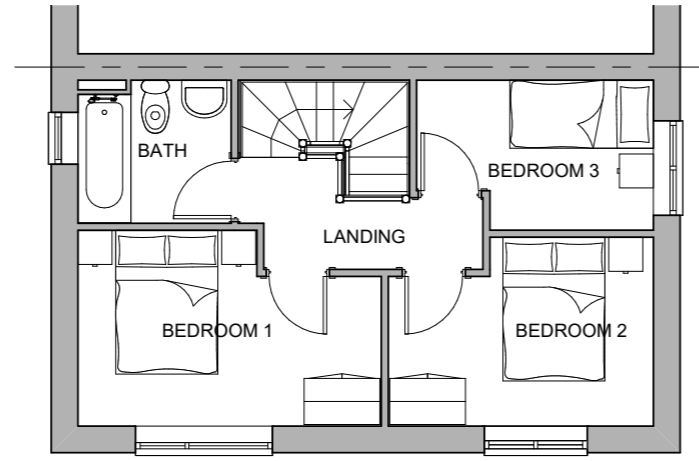




AREAS		
FLOOR	AREA m ²	AREA ft ²
GROUND	35.4 m ²	381 ft ²
FIRST	35.4 m ²	381 ft ²
TOTAL	70.8 m²	762 ft²



Ground Floor Plan



First Floor Plan



Front Elevation





Rear Elevation

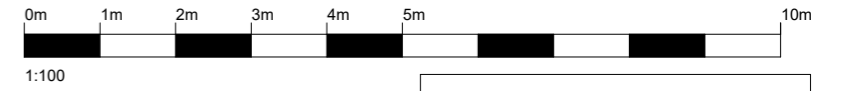


Side Elevation

Rev.	Date	by	Description
B	14/10/20	MS	Verge caps added
A	26/02/20	DH	Canopy amended

Z:\Jobs\2019 Jobs\18-122 Great Places - House Type Range\Drawings\proposed\plans\Working Drawings\Co-Alt.rvt

Client: 	Project: Core House Types	Drawing Title: Cornbrook Alt 762 Sqft. Code - Co Alt	Drawn: AF Checked: DH Scale (A3) : 1 : 100 Date: 26/11/19 Drawing No: CO-ALT-001 Job No: 18-122 Rev: B	<p>CONSTRUCTION</p>	General notes: Do not scale the drawing. All dimensions to be checked on site prior to commencement of work and any discrepancy shall be immediately reported and resolved prior to work commencing, this drawing is to be read in conjunction with all relevant drawings and specifications relating to the job whether or not indicated on the drawing. copyright reserved to mck associates ltd. and this drawing may not be used or reproduced without prior written consent.	
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Area Schedule (GIA)		
Name	AREA m ²	AREA ft ²
GROUND	57 m ²	609 ft ²
FIRST	55 m ²	592 ft ²
TOTAL	112 m²	1201 ft²



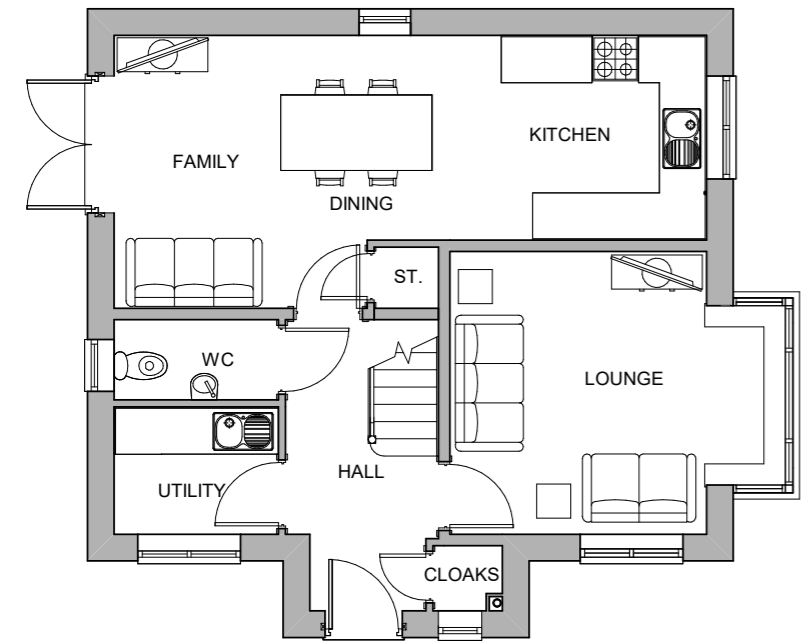
Front Elevation



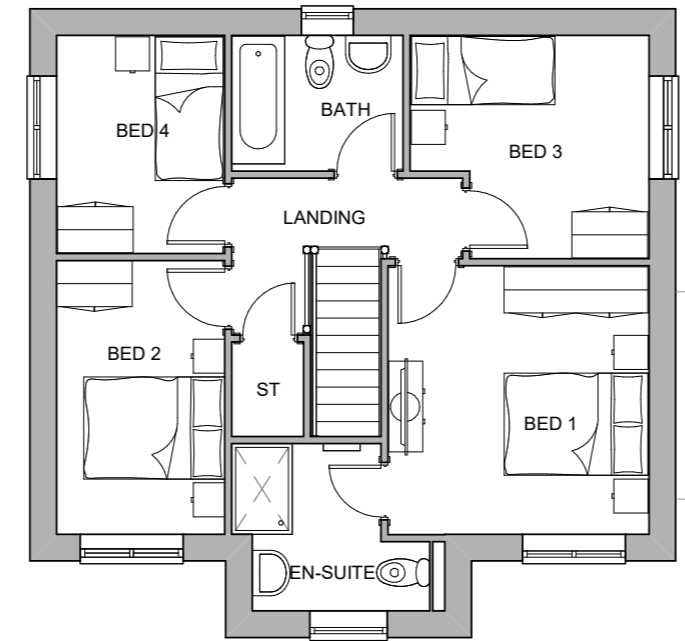
Side Elevation 1



Rear Elevation



Ground Floor Plan



First Floor Plan

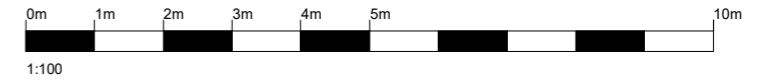


Side Elevation 2

Rev.	Date	by	Description
C	21/20/20	MS	Layout amended
B	30/04/20	GO	Kitchen unit amended
A	30/04/20	DH/HR	Layout amended

Z:\Jobs\2018\18-122 Great Places - House Type Range\Drawings\proposed\plans\Working Drawings\Br Alt B.rvt

Client:	Project: Core House Types	Drawing Title: Brooklands Alternative Bay 1201 Sqft. Code - Br Alt B	Drawn: DH Checked: SK	Scale (A3) : 1 : 100 Job No: 18-122	Date: 6/3/2020 Rev: C	General notes: Do not scale the drawing. All dimensions to be checked on site prior to commencement of work and any discrepancy shall be immediately reported and resolved prior to work commencing. This drawing is to be read in conjunction with all relevant drawings and specifications relating to the job whether or not indicated on the drawing. Copyright reserved to mck associates ltd. and this drawing may not be used or reproduced without prior written consent.	
PLANNING							



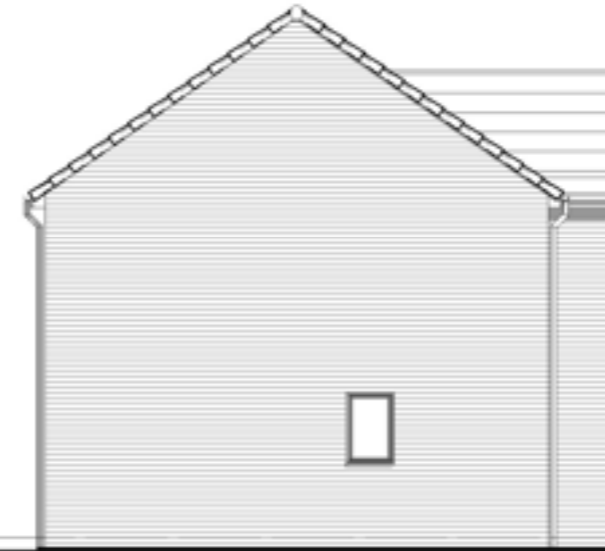
AREA SCHEDULE		
FLOOR	AREA m ²	AREA ft ²
GROUND	55 m ²	592 ft ²
FIRST	55 m ²	592 ft ²
TOTAL	110 m ²	1185 ft ²



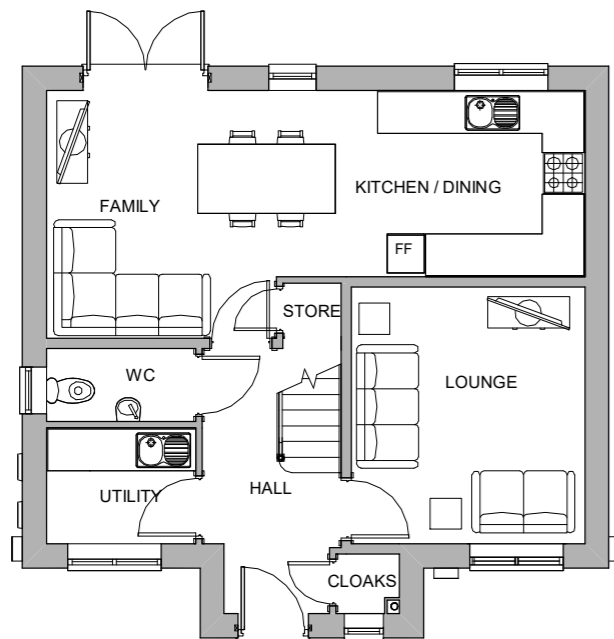
Front Elevation



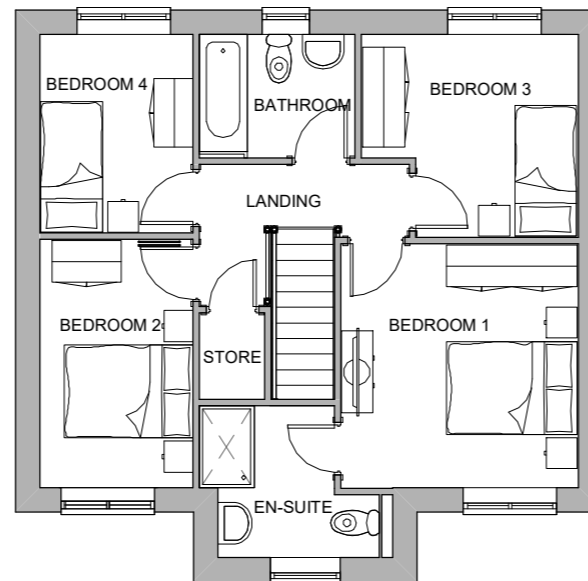
Rear Elevation



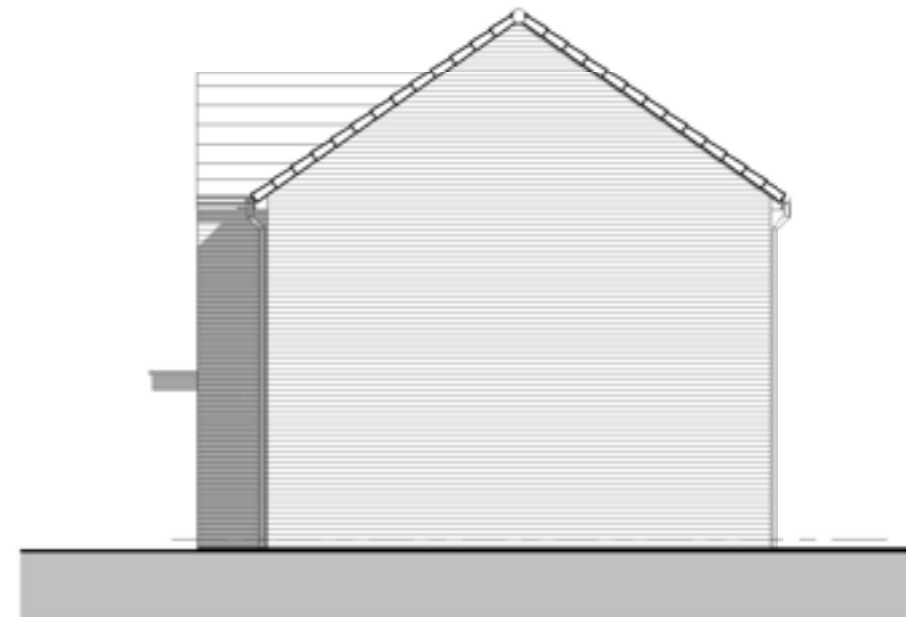
Side Elevation 1



Ground Floor



First Floor



Side Elevation 2

Rev.	Date	by	Description
B	17/08/22	JS	W3 raised by 150mm
A	30/04/20	GO	Kitchen unit amended

Z:\Jobs\HIVE\HIVE Homes - Master Drawing Pack - March 2022 (Previously June 2021) BR - (Brooklands) BR.dwg

Client:



Project:
Core House Types

Drawing Title:
Brooklands
1185 Sqft.
Code - Br

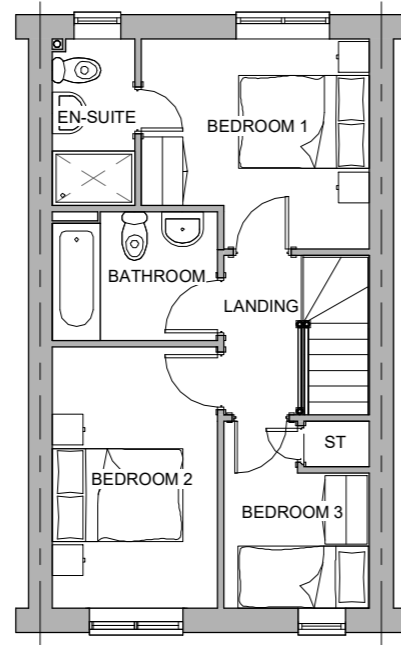
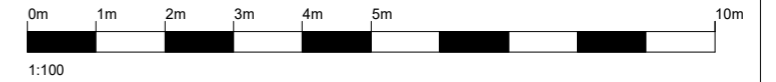
Drawn: DH
Checked: SK
Scale (A3): 1 : 100
Date: 20/06/19

Drawing No: BR-001
Job No: 18-122
Rev: B

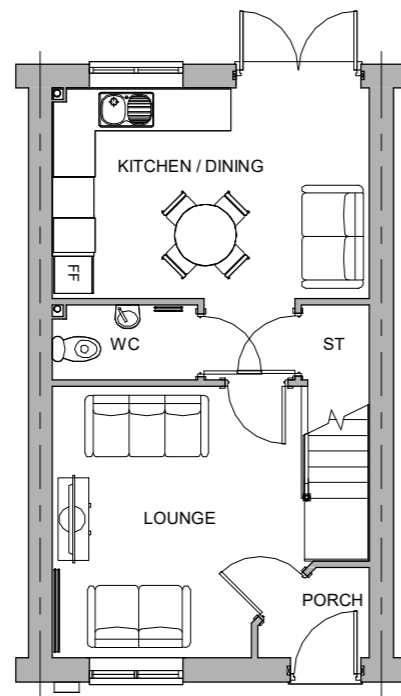
CONSTRUCTION

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First Floor



Ground Floor



Front Elevation



Rear Elevation

AREA SCHEDULE		
FLOOR	AREA m ²	AREA ft ²
GROUND	39 m ²	418 ft ²
FIRST	39 m ²	418 ft ²
TOTAL	78 m²	837 ft²

Rev.	Date	by	Description
C	17/08/22	JS	W1 raised by 150mm
B	14/10/20	MS	External lighting and tap added
A	27/02/20	DH	Canopy altered

Z:\Jobs\HIVE\HIVE Homes - Master Drawing Pack - March 2022 (Previously June 2021)\BO-MID - (Bowker Mid)\Bo Mid.rvt

Client:



Project:

Core House Types

Drawing Title:

Bowker Mid
837 Sqft.
Code - Bo Mid

Drawn: DH
Checked: SK
Scale (A3): 1 : 100
Date: 26/11/19

Drawing No: BO-MID-001
Job No: 18-122
Rev: C

CONSTRUCTION

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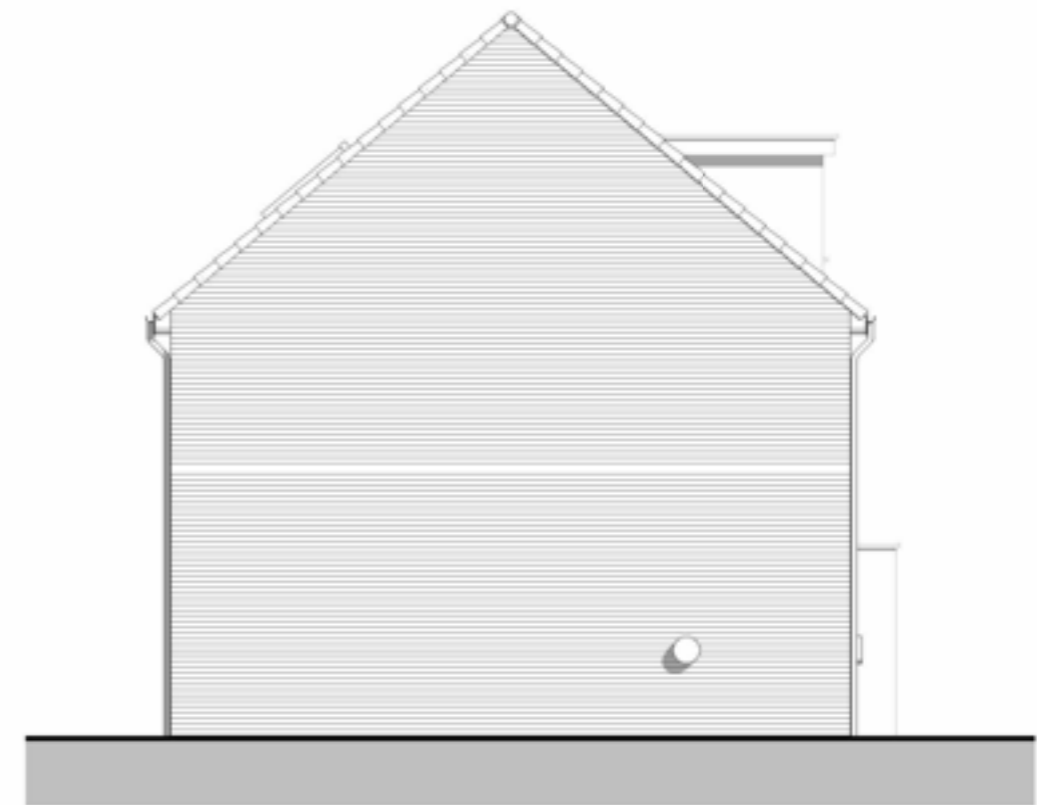
AREAS		
FLOOR	AREA m ²	AREA ft ²
SECOND	27.0 m ²	291 ft ²
GROUND	39.0 m ²	420 ft ²
FIRST	39.0 m ²	420 ft ²
TOTAL	105.0 m²	1131 ft²



Front Elevation



Rear Elevation



Side Elevation

A	15/10/20	MS	External lighting and tap added
Rev.	Date	by	Description

Client:



Project:

Core House Types

Drawing Title:

Newbold End
1131 Sqft.
Code - Nb End

Drawn:

DH

Checked:

SK

Scale (A3):

1 : 100

Date:

20/06/19

Drawing No:

NB-END-002

Job No:

18-122

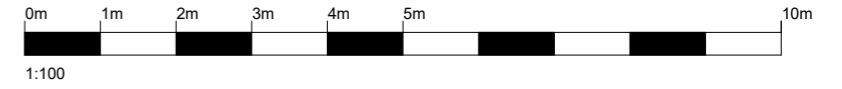
Rev:

A

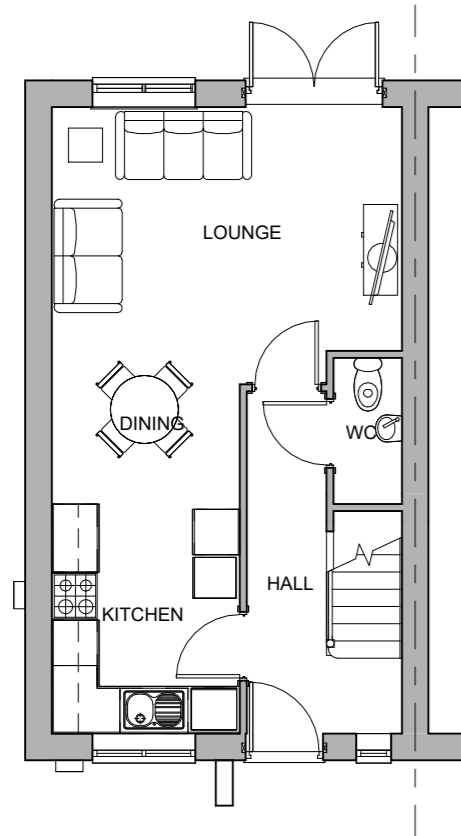
PLANNING

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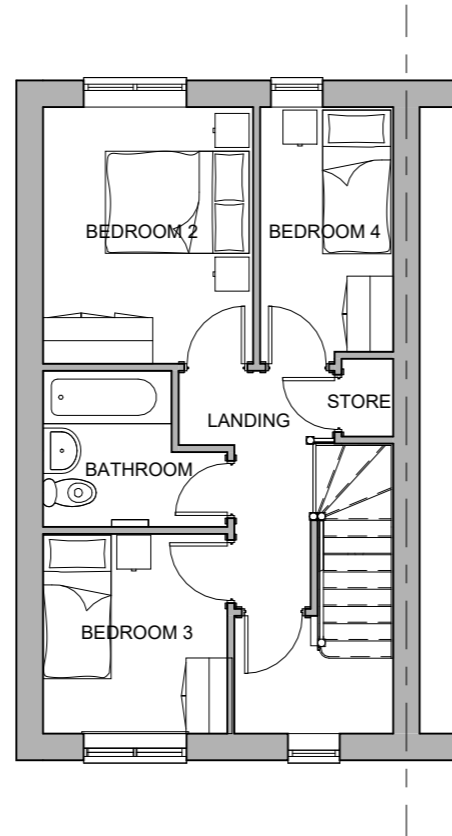




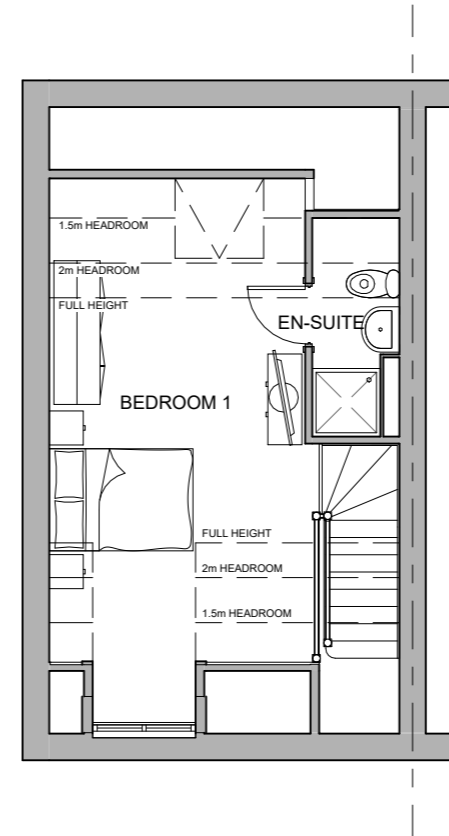
AREAS		
FLOOR	AREA m ²	AREA ft ²
SECOND	27.0 m ²	291 ft ²
GROUND	39.0 m ²	420 ft ²
FIRST	39.0 m ²	420 ft ²
TOTAL	105.0 m²	1131 ft²



Ground Floor



First Floor



Second Floor

A	21/04/20	GO	kitchen unit & duct amendment
Rev.	Date	by	Description

Client:



Project:
Core House Types

Drawing Title:
Newbold End
1131 Sqft.
Code - Nb End

Drawn: DH
Checked: SK
Scale (A3): 1 : 100
Date: 20/06/19

Drawing No: NB-END-001
Job No: 18-122
Rev: A

PLANNING

General notes:
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